

March 20, 2024

Dear Beachcomber Inn HOA member:

The attached project programming report was issued by Lumos & Associates (a consulting firm located in South Lake Tahoe, CA) at the request of the Beachcomber Inn Board of Directors. This report was generated after Lumos & Associates evaluated the Beachcomber site per the scope specified by the Board. (See the Project Scope section below for details.) This project programming report is intended to assist the Board in evaluating the feasibility and priority of various infrastructure improvements at the Beachcomber Inn.

PROPERTY HISTORY

The original buildings and associated infrastructure that make up the Beachcomber Inn were initially built in 1958. The site was originally designed and built as an apartment building located on the shore of Lake Tahoe and then was later converted to a motel. In 1981 the site and its associated buildings were reconfigured and converted into a timeshare resort and named the Beachcomber Inn. Over the following years the homeowner association (HOA) for the Beachcomber Inn has maintained the buildings and amenities, but key infrastructure components were never rebuilt (except for the dock/pier and sea wall that were rebuilt in 2008). The only major amenity added by the HOA after the property became a timeshare resort was the upper and lower elevators in 2001. In 2023, the lower-level elevator was determined to be unreparable and is now permanently decommissioned.

INTENDED PROJECT SCOPE

The original project scope for the Exterior Remodel Project, as identified in 2020, was to rebuild the parking lot and to remodel / update the exterior of the buildings. When the Board reviewed the property and began drafting the project requirements, it became apparent that there were other infrastructure projects that needed to be evaluated and prioritized along with the parking lot and the building exteriors. The original project feasibility study was then expanded to evaluate the following areas:

1. **Structural Evaluation:** Evaluate the following onsite structural items and, where appropriate, determine the current structural condition of each one:
 - Carport beams and support elements
 - Various decks and walkway railings
 - Observation deck area and all associated attachments and surfaces below it.
 - Elevator supporting walls.
 - Retaining wall on south side of Building A-J.
 - Planter box retaining walls throughout the site.
 - Feasibility of adding stairs at the west end of the A-J retaining wall.
 - Investigate ways to expand the front entry Portico.
 - Evaluate the structural integrity of the front carport elements.
 - Propose possible repair/replacement options for the Building A-J retaining wall.

2. Site Improvements Programming

Summarize the scope of work for site structural improvements, including impacts and recommendations related to parking, pedestrian and vehicular circulation, ADA accessible routes, site grading and drainage, utilities, BMPs, well abandonment, and associated items of work. Additionally, land coverage and capability classifications, as determined by TRPA, will be reviewed. Identify key design components and provide an engineer's opinion of possible costs based upon very rough preliminary estimates.

3. Architectural Programming

The architectural analysis will be conducted by Monarch Architecture (a third-party consultant located in South Lake Tahoe, CA). Monarch Architecture will study the current structure, architecture, and function of the resort grounds to determine where deficiencies and opportunities may exist. Additionally, Monarch will identify the items that need to be brought up to current building codes during the remodel process. The analysis will include ADA accessibility within the building, occupancy restrictions, and egress requirements.

The architect will also investigate the feasibility of the following possible building modifications:

- Expansion of portico over the front entry.
- Updated building façade including facias, siding, windows, and doors.
- Updated and compliant exterior lighting and signage.
- Updated and compliant stair and deck railings.
- Required code upgrades based on the site findings.

PROJECT PLANNING REPORT CONTENT

The attached programming report summarizes the conclusions reached by Lumos & Associates and Monarch Architecture. It covers two of the three major infrastructure components of the Beachcomber Inn site (i.e. the site building exterior makeover and the site retaining walls). The detailed programming for the Parking Lot replacement is not included in the programming report because only a very limited programming analysis has been done. Lumos is currently working on the parking lot assessment, which includes in-filling the well, replacement of the sewer and storm drains, and releveling and paving the parking areas. (A rough cost estimate for the parking lot repair was included in Appendix A but may be unrealistically high.)

COST ESTIMATES

Appendix A provides very "rough" cost estimates associated with the various improvements proposed in the Programming Report. Please disregard the magnitude of the costs shown. The cost estimate chart should only be used for comparing the relative cost differences between various improvements. These cost estimates were made in April 2023 with minimal definition of engineering design. For example, the initial cost shown for replacing the retaining behind the A-J Building was estimated at \$1,600,000. The recent versions of the proposed retaining wall options may have a more realistic cost estimate of \$300,000 - \$500,000. This revised cost may decrease even further when the specification and design for the retaining wall is finalized.

CONCLUSIONS

Due to the large amount of information in the report, it may be overwhelming for a person reading it for the first time. Several points to keep in mind as you read the report are:

- The buildings and site were constructed in the 1960's and are now over 60 years old. Very few repairs to the major site infrastructure items have been made since the property was originally built.
- As mentioned above, DO NOT focus on the cost estimates as the pricing may decrease as we better define the final scope and develop the actual specifications for the remodel project.
- This report is an assessment of the current condition of the infrastructure of the property. Not all items listed in the report are in urgent need of repair/replacement. Additionally, we can choose to implement some suggestions over time or choose not to implement some of them at all.
- The Beachcomber Inn is a very unique resort since it is the only timeshare resort located on the water's edge of Lake Tahoe, with its own swimming pool, beach, pier, and boat moorings.
- The Board will be presenting additional information and answering questions about the Exterior Remodel Project at the upcoming Annual Beachcomber Homeowners Meeting on Saturday, April 6, 2024 @ 10:00 AM PST. All Homeowners are encouraged to either attend the meeting in-person in Sacramento CA or to attend by using the ZOOM meeting login. The Board looks forward to your comments. If you have any questions about the Annual Meeting, please contact the Beachcomber Inn office at (530) 544-2426.

Regards,

Board of Directors
Beachcomber Inn Vacation Membership Association

PROGRAMMING REPORT
For
Beachcomber Inn Resort

Prepared For:

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APPENDICES

Appendix A: Preliminary Opinion of Probably Costs

Appendix B: FEMA National Flood Hazard Layer FIRMette

Appendix C: *Structural Investigation Summary and Structural Evaluation Report*, Lumos & Associates, dated April 25, 2023

EXECUTIVE SUMMARY

Lumos & Associates and Monarch Architecture have partnered to provide a summary of the general requirements, constraints, permitting jurisdictions, and budgetary cost opinions for the proposed capital improvement projects list provided by the Beachcomber Inn Vacation Membership Association. Our team conducted a site walk with the owners' representatives to make visual observations and discuss the project approach from a project planning and budgetary perspective. This report outlines the sequence of events and general implications of each proposed item of work. Based upon the proposed improvements and the findings in this report, we recommend that the Association prioritizes projects considering safety concerns, added value, and cost of each project. This report is also intended to help the Association determine the amount and timeline required for Special Assessment and/or construction loan funding. If budget constraints require the project to be phased, we recommend prioritizing the retaining wall ramp for ADA accessibility compliance. It is assumed that the site will also need to be brought into BMP compliance with the first phase of construction. Our team would be happy to meet with the Association to help prioritize items of work to work and discuss architectural alternatives discussed in this report.

1. INTRODUCTION

1.1. Background and Scope

The Beachcomber Inn Vacation Membership Association, also referred to as the Homeowners Association (HOA), is exploring potential upgrades to The Beachcomber Inn Resort (Beachcomber) under a Special Assessment. The Beachcomber is a lakefront multi-family timeshare condominium property, including 22 accommodation units and various guest amenities. It is located at 999 Lakeview Avenue in South Lake Tahoe, California. See Figure 1 for a location map of the Beachcomber.

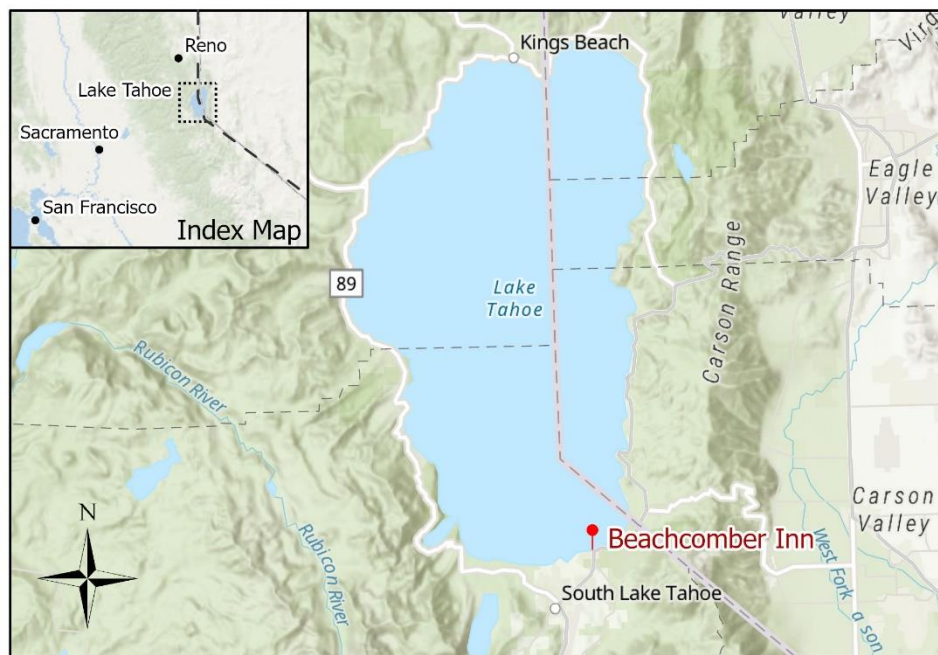


Figure 1. Location Map

Lumos and Associates, Inc. (Lumos) has partnered with Monarch Architecture to provide this engineering and architectural Programming Report based on the proposed site and building improvements being proposed by the Beachcomber Inn Vacation Membership Association (the Association). We will also be working in conjunction with Exline & Company, contracted separately with the Association, to coordinate Planning items pertaining to permitting, entitlements, and sequencing. The intent of this Programming Report is to assist the Association in understanding the potential project costs, schedule, and permitting implications for the proposed improvements on the Special Assessments project list provided to us. Additional concerns, general

recommendations, and any other pertinent information are also presented for your project planning considerations. Please note that no landscape architecture analysis or geotechnical investigation are included in the programming scope. Lumos will be able to include geotechnical investigation in a future scope of services, as it will be required for the design and permitting of the proposed improvements.

A separate Structural investigation and evaluation report accompanies this Programming Report, attached as Appendix C, which evaluates the structural condition of existing structural elements associated with the carport, deck, walkway railing attachments, elevator support walls, retaining walls, and expanding the existing porte cochere. Recommendations for construction methodology around the retaining wall are also presented.

1.2. Special Assessment Project Considerations

A list of potential site upgrades as identified by the Association under the Special Assessment Projects list is outlined below. All of these identified items of work are not necessarily recommended, but are discussed in subsequent sections of this report.

1. Parking Lot, Utilities, & Landscaping

- Remove ~8500-9000 SF of asphalt and haul away.
- Remove and haul away raised planter and sign at SE corner of lot.
- Remove select trees and landscaping to increase asphalt coverage along front.
- Extend porte cochere to sidewalk edge.
- Rebuild sewer and water lines as necessary under parking lot area. Relocate main water valve into Christy box below grade.
- Build low stone wall along sidewalk. ~3-4' high.
- Add another driveway access to carports (if allowed).
- Decommission and backfill well. Remediate and backfill any discovered septic tanks.
- Build electric snowmelt hotpad + drainage system in parking lot.
- Regrade/rebuild parking area to meet BMP and ADA standards.
- Repave parking areas with 3" AC over 6" compacted aggregate base.
- Concrete pave carports, graded to meet ADA access into Unit L.

2. Retaining Wall

- Remove existing walls and rebuild per design plans.
- Lay slope back with a stepped system. Add concrete stairwell on west end of wall.

3. Building Exterior Upgrades

- Add façade per architectural plans (forthcoming).
- Check structural capacities of beams over the carport. Shore as necessary.
- Rebuild railings.

4. Common Deck
 - Check structural capacity of deck support system. Reinforce and rebuild as necessary.
5. Beach
 - Add more riprap along sheet pile wall to dissipate wave energy and prevent loss of sand.

Known implications, recommendations, and permitting requirements for the site improvements listed above follow in Section 3 – Project Planning and Permitting. Figure 2, below, provides an overview of the various potential projects considered in relation to their general location on the property.



Figure 2. Overview of Potential Special Assessment Projects

2. EXISTING CONDITIONS

2.1. Property Information

The Beachcomber is a multi-family residential development, located on a single parcel at 999 Lakeview Avenue, South Lake Tahoe, CA 96150. The parcel is 0.79 acres in total (+/-34,412 square-feet (SF)) and is characterized as a lakefront property within the Al Tahoe Amended Subdivision, located in the Lake Tahoe Basin. It is situated west of the Lakeview Commons public beach, on the northwest corner of the intersection of Lakeview Avenue and Harrison Avenue (see Figure 3). General property detail are outlined below.

- APN: 026-043-015
- Section, Township, Range: S34, T3, R69
- 35,020 SF or 0.79 Acres
- Four (4) existing buildings
- Year of original building construction: 1958
- 22 timeshare units, including 17 suites, four (4) studios, and one (1) accessible unit
- A private boat dock, seasonal pool and spa, and common deck/barbeque area
- 22 uncovered parking spaces, including 1 accessible parking space

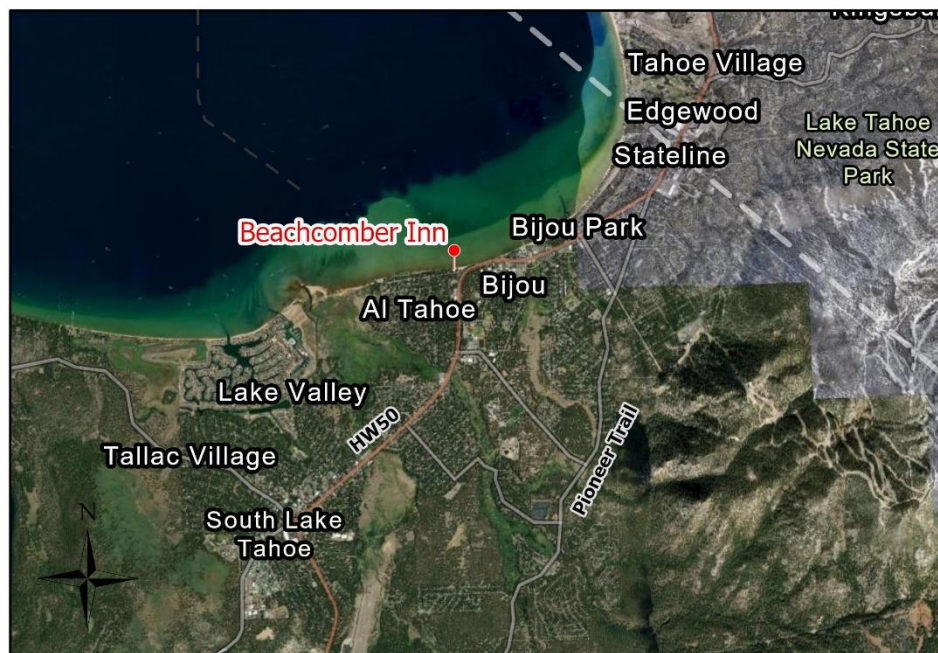


Figure 3. Site Vicinity Map.

2.2. Site Description

The Beachcomber is a fully built-out multi-family development situated on the shoreline of the Lake Tahoe within the Bijou Creek Watershed. Its beachfront area is directly west of the Lakeview Commons public beach and is accessible to the public. The property is outside of any floodplains and is designated by FEMA as an 'Area of Minimal Flood Hazard' (see Appendix B). Its buildings and communal spaces were constructed on a naturally steep slope of approximately 10-15% on average, resulting in the structures having a terraced layout stepping down from the roadway towards to the shoreline. Most of the common area on the property is paved to accommodate parking and pathways between units and communal spaces. Compliance of existing accessible routes with the Americans with Disabilities Act (ADA) have not been confirmed as part of this investigation and would require topographic survey data. There are some existing landscape areas on the site that are relatively small and sparsely located. Several mature pine trees are also located around the property.

2.3. Agencies and Authorities

Due to the Beachcomber's location within the Lake Tahoe Basin in South Lake Tahoe, California, it currently falls under the jurisdiction of the following agencies:

- Planning → Tahoe Regional Planning Agency (TRPA)
→ City of South Lake Tahoe (CSLT)
- Fire Protection → CLST Fire Department
- Environmental → TRPA
- Water and Sewer → South Tahoe Public Utility District (STPUD)
- Storm Drainage and Roads → CSLT
- Building and Engineering → CLST
- Electricity → Liberty Utilities
- Gas → Southwest Gas Company (SW Gas)
- Communications → Spectrum (a.k.a. Charter)

The proposed site improvements do not pose any impact to State highway right-of-way, so permitting through California Department of Transportation (Caltrans) will not apply. Based on the preliminary scope of work, we also do not anticipate that these projects will require permitting through the California Environmental Quality Act (CEQA) or Federal Emergency Management Agency (FEMA). However, if work is pursued in the backshore zone of Lake Tahoe, additional environmental permitting will apply that should be coordinated with the Planner. Based on conversations with Exline & Company, it is recommended that beach erosion stabilization be treated as a standalone project. Therefore, the primary permitting agencies for the proposed site

improvements will be the City of South Lake Tahoe and TRPA. The extents of TRPA permitting is deferred to the Programming Report prepared by the planning consultant, Exline & Company.

3. PROJECT PLANNING & PERMITTING

3.1. Civil Site Assessment & Considerations

Considerations pertaining to civil site improvements listed by the HOA's preliminary Special Assessment Projects outlined in Section 1.2 are discussed in this section. The proposed SAP items of work are broken down into categories with a summary of implications and recommendations for each.

3.1.1. Parking Lot Improvements

Lumos concurs with the HOA's assessment that the existing asphalt and concrete parking lot surface should be replaced at full depth. It is recommended to remove the existing surface pavement, aggregate base to a depth that will accommodate a new pavement section recommended by subsequent geotechnical investigations. Any increase or relocation of land coverage will require a land coverage survey and verification through TRPA. Parking lot improvements will need to be graded to drain to proposed BMPs for infiltration on site.

The HOA also expressed an interest in installing an electric "hotpad" snowmelt and drainage system in the parking lot. Based on the limited space, a designated snowmelt area is not advisable. However, depending on available HOA funding, a heated parking lot would be encouraged to help mitigate the lack of snow storage area on site. Snowmelt from the heated pavement would drain to proposed permanent BMPs for infiltration, in the same fashion as storm water, as discussed below in Section 3.1.2 BMP Requirements.

With the limited parking on site, we understand that there is a desire to increase the number of parking stalls if possible. With the removal of two large trees along the east side of the office, there may be an opportunity for an additional parking stall. Topographic survey will be required in order to verify dimensions, drive aisle widths, and confirm the achievable parking stall count.

3.1.2. BMP Requirements

Any work that requires permitting through the City of South Lake Tahoe (CSLT) or TRPA will require the site to be brought into BMP compliance with TRPA. This means that BMP measures need to be designed to infiltrate all surface water generated from the design storm into the ground before leaving the site. It is expected that this will require gravel infiltration trenches, underground infiltration galleries, and/or infiltration wells. Calculations will need to be provided on the design plans demonstrating that the proposed BMP measures are in compliance with TRPA's infiltration requirements. According to TRPA's Parcel Tracker, a BMP Certificate has not

been issues for this site. However, the Parcel Tracker website identifies an infiltration rate, or (K_{Sat}) value, of 12.8 in/hr , indicating favorable soil conditions for storm water infiltration.

With this project, we recommend replacing the existing dry wells in the parking lot with drainage inlets to direct storm water to underground infiltration galleries located under the parking lot. In any unpaved areas under roof driplines, and along edges of pavement, we would propose to install gravel infiltration trenches. The recommended BMPs would be consistent with TRPA standards, focusing on those with ease of maintenance.

3.1.3. Land Coverage

The amount of existing land coverage will need to be verified through a topographic survey. The land coverage map and calculations then get submitted to TRPA for land capability and coverage verification, which determines the amount of allowable coverage for the property based on the designated land capability. If TRPA determines that the site already exceeds the amount of allowable coverage, the amount of existing coverage on site typically governs. In that case, no net increase in land coverage would be permissible. However, coverage could be relocated or reallocated across the site. We suspect that the site is currently exceeding its allowable coverage.

3.1.4. ADA Accessibility

With the proposed parking lot and site improvements, ADA accessible routes will need to be provided from the handicap parking stall(s) to the accessible condo unit (Unit L), as well as the front office door, public sidewalk along the frontage, and all common areas and amenities. Topographic survey will be required to develop design plans and will allow us to identify any non-compliance with ADA accessibility requirements.

It is our understanding that ADA accessibility to the lower level units and pool level is provided through the use of elevators, which have been subject to extensive maintenance, costly repairs, and inaccessibility issues. Therefore, we have explored the possibility of building an ADA-compliant ramp from the walkway behind the upper building down to the concrete walkway at the lower building, which involves three switchbacks. This appears to be a feasible option for eliminating the elevator system from the common deck down to the middle level.

3.1.5. Driveway Configuration

Based on driveway spacing requirements, we do not believe a third driveway approach would be permitted by the City for improved access to the carports. The two existing driveways seem to be situated in the most practical locations for site access, emergency vehicle maneuvering, and overall functionality of the site. Shifting either of the driveway approaches closer to the center of the site would limit possibilities for expansion of the porte cochere and parking near the front office.

Access for refuse vehicles to the trash enclosure is another consideration for driveway layout. The minimum trash enclosure size required by the City is expected to have inside opening dimensions of 11'4" wide by 8' deep with double swing gates and a solid enclosure wall. An 8-foot deep paved apron is also typically required, matching the width of the enclosure. Due to this requirement, the trash enclosure will take up approximately one parking space, wherever it is situated on site. Recommended locations are 1) in its current location 2) in the first parking stall behind the existing monument sign at the southeast corner of the site, or 3) where the existing tree is located off the southeast corner of the office. It was discussed that the HOA may want to remove this tree anyway due to its proximity to building.

3.1.6. Tree Removal

Removal of trees greater than 14-inch diameter at breast height needs to be shown on plans and approved by TRPA. Two large pine trees exist along the east face of the office/front building that are in close proximity to the structure and could pose safety and/or fire risk. In order to construct a trash enclosure in the dumpster's current location, removal of a third existing pine tree at the southwest corner of the site would likely be required. Additional trees along the east property line should be evaluated for their risk related to the location of the easternmost building and power lines. While mitigating risk, removal of these two trees could also allow more room for parking or BMP features.

3.1.7. Stone Landscape Wall

The HOA expressed a desire to replace the existing decorative fence along the road right-of-way frontage with a low stone wall, approximately three to four feet tall, due to the frequent damage caused to the fence by snow plow operations. We agree that this is a good alternative to reduce maintenance and repair costs, which will also allow for beautification of the entrance to the property.

3.1.8. Utility Work

It was conveyed to us that there may be a sag, or belly, in the existing sanitary sewer lateral in the parking lot alongside the office in the main building, requiring pipe clearing on occasion. We recommend exposing the sewer lateral while the parking lot pavement is being replaced to assess the condition of the pipe. If the pipe is in good condition, it can be re-laid at a constant slope to the sewer main. However, if it is determined that the pipe is beyond its useful life, it should be replaced with new pipe in its place. In any case, proper pipe bedding, backfill, and compaction should be used per STPUD standard details and specifications.

We understand that a sanitary lift station serves the lower residential units and has been operating with no issues. Therefore, Lumos has not included any inspection, analysis, or recommendations pertaining to the sanitary lift station as part of this scope.

The HOA has expressed a desire to relocate the existing domestic water backflow preventer from the above-grade enclosure adjacent to the raised landscape bed at the southeast corner of the site to an underground vault. This will not be permissible by the South Tahoe Public Utility District (STPUD) as they require backflow preventers to be in an enclosure above grade. Based on the condition of the existing enclosure, we recommend replacing the backflow preventer with a Hot Box style enclosure in its current location and protecting it from vehicular traffic with bollards, boulders, or other TRPA-approved barrier.

Well Abandonment

An existing domestic water well is situated in the eastern portion of the parking lot. According to HOA representatives, the well is no longer in service and the buildings are served by a municipal water connection. The HOA has inquired about potentially abandoning the well. The scope of proposed work for the well abandonment and materials used to seal the well would need to be approved under the Application for Well Permit through the El Dorado County Environmental Management Department. Well abandonment would need to be performed by a certified well driller with a C-57 license to do the demolition. Abandonment procedures typically range from remove of the upper five feet of the well structure and backfilling with approved materials to full depth well abandonment, equipment removal, remediation, and backfill, dependent on the conditions of the well and the site.

Septic System Abandonment

It is unknown if any septic systems exist on site. However, if any are discovered and need to be removed, this work would also need to be permitted and monitored by the El Dorado County Environmental Management Department. Methods for removal and remediation would be dependent on what is encountered upon excavation.

Riprap Shore Protection

Upon our site visit, the HOA pointed out the beach erosion that has taken place since installing the sheet pile wall along the foundation of the patios facing the lake at the lower level building. It was discussed that wave energy dissipation could help mitigate the water turbulence and drawback of sand from the beach. Large diameter riprap is a common method for wave energy dissipation along shorelines and would seem to be an appropriate application for this situation, subject to TRPA approval. We defer to the Planner regarding permitting requirements for work in the shore zone, and would recommend treating such work as a standalone project due to the regulatory complexities involved.

3.2. Structural Assessments & Recommendations

Concurrently with this report, Lumos & Associates conducted a structural site investigation and evaluation focused on key components identified by the HOA, exclusive of architectural components. The findings of that investigation are discussed in a separate report, attached as Appendix C, titled *Structural Investigation Summary and Structural Evaluation Report*.

In summary, the referenced report addresses the existing conditions of the carport and supporting elements, common deck area, walkway railing attachments, elevator support walls, stepped retaining walls, and existing porte cochere. The report also includes a section regarding the feasibility of installing a new retaining wall system with integral ADA ramp in an effort to eliminate the need for elevators to meet ADA accessibility requirements. Note that recommendations provided in the Structural Report do not imply conformance with TRPA scenic assessment requirements or local architectural requirements as they pertain to aesthetics. Selected improvements will need to be coordinated with the permitting agencies through the Planning consultant.

3.3. Programming for Civil Site Improvements

This section outlines the general design and key permitting considerations, impacts and recommendations related to parking, pedestrian and vehicular circulation, ADA accessible routes, site grading and drainage, utilities, BMPs, well abandonment, and associated items of work. Please refer to the supplemental "Due Diligence Evaluation and Report" prepared for this property by Exline & Company, dated February 23, 2023, for a detailed outline of permitting requirements and agency jurisdiction over proposed items of work. The Exline & Company report was prepared for the HOA under a separate agreement.

3.3.1. Design Components

Design elements required to produce drawings for permitting of the Special Assessment projects as they relate to Site Planning include the following:

- Topographic survey with boundary, surface, and manhole dips
- Pavement section design
- Site grading and drainage design
- Site layout design for walkways and parking lot
- BMP design and calculations for storm water infiltration
- Water and sewer service design
- Landscape and irrigation design
- Geotechnical investigation & Soils Hydrology investigation (TRPA)
- Structural design for foundations and structural components
- Architectural design for building modifications

3.3.2. Permitting Jurisdictions & Design Considerations

City of South Lake Tahoe (CSLT):

- Pedestrian walkways and handicap parking stall access will need to be designed to comply with accessibility standards set forth by the California Building Code, which takes precedent over the federal ADA standards. The CSLT will review accessibility design under their permit review process. Accessible walkways need to be provided to all common areas and shared facilities on site. It is recommended that the existing accessible route from the handicap parking stall to the accessible condominium unit be analyzed for compliance. A new accessible walkway will need to be provided to the front office doorway with the proposed parking lot and porte cochere improvements.
- To improve ADA accessibility to the lower level building and pool area, it is recommended that an ADA compliant ramp be designed and constructed at the entrance gate to the pool, where there is currently a step. We also recommend considering a ramp system to replace the existing retaining wall between the upper and lower buildings that would provide ADA-compliant access to the pool level. The layout and recommendations for this ramp are discussed in the Structural Report included in Appendix C – Figure 1. This ramp would eliminate the need for the existing elevator from the common deck to the pool level.
- As outlined in the Due Diligence Report from Exline & Company, the minimum setbacks are 20 feet for front yard, 10 feet for interior side yards, and 15 feet for rear yard. It appears that the existing porte cochere currently violates the front yard setback. Therefore, further encroachment into the setback with porte cochere modifications will likely not be permitted. However, reconstruction and modifications can be evaluated as coordinated with the HOA.
- Required off-street parking is not allowed within the setbacks for residential development larger than a triplex.
- Significant modifications to the driveway and parking lot layout will require the parking lot to be brought into conformance with the CSLT's standards as well as the California Fire Code for emergency vehicle accessibility. Two-way driveways shall have a minimum width of 20 feet and a maximum width of 30 feet. If the driveways were converted to one-way driveways, the minimum widths would be 10 feet with a maximum width of 15 feet. Unless a plan is devised that will present significant improvement to vehicular circulation or parking, it is recommended that reconstruction of the parking lot be limited to matching existing conditions in order to avoid additional requirements being imposed to bring the parking lot up to current standards that may not be feasible for the scope of the project.

- Layout modifications to the driveways or parking area will need to comply with Fire Department regulations for emergency vehicle access. This is typically routed to the Fire Department via CSLT during permit reviews processes. Although it may not be required by Code due to drive aisle length less than 150 feet and less than 100 units, we recommend maintaining two driveways to maintain efficient vehicular circulation. The minimum drive aisle width required for fire apparatus access is 20 feet. Once topographic survey is completed, we will be able to determine whether the existing parking lot layout satisfies CSLT and Fire Code requirements.
- There were discussions on site about potentially relocating the western driveway for carport accessibility. However, the current driveway location appears to provide the most efficient use of drive aisle space for parking underneath the porte cochere. We recommend maintaining both driveway locations.
- Properties categorized as Tourist Accommodation have a minimum parking requirement of 1 space per guest room or unit, and 1 space per 6 employees. Fractions of calculated parking spaces less than 0.5 can be disregarded, where fractions of 0.5 or greater shall be counted as an additional parking space for purposes of determining the minimum number of parking spaces required. Based on 22 units and 2 employees, the minimum number of parking spaces is 22 units, which matches the existing number of spaces on site (includes one ADA accessible space).
- Major repairs and improvements will require a snow storage plan to be shown on the construction drawings, subject to City approval. Designated snow storage areas may not be located within required parking spaces. The limited snow storage area on this property has presented challenges to the owners and property management staff. For these reasons, we recommend maintaining the maximum width of landscape area available along the back of the public sidewalk and minimizing changes that may trigger compliance with the City's snow storage requirements.
 - Heated parking lot pavement could help minimize snow storage area requirements, but would be subject to City approval.
 - A snow melt system could also be considered in the exterior corridor outside of the lower building and within any pedestrian ramp constructed in that vicinity. There appear to be drains in the exterior concrete slab available to drain snow melt away from the structure.
- Any proposed retaining wall greater than three feet high will require structural design and will need to be permitted through the CSLT Engineering Department.
- Because this site is located within the City limits TRPA permit applications will need to be submitted to the City to either review on TRPA's behalf, or for the City to make a determination that TRPA will perform their own reviews based on the nature of the proposed work.

Tahoe Regional Planning Agency (TRPA):

- Re-paved, areas will need to be graded to drain towards existing drainage facilities with a proposed peak discharge no greater than the existing peak discharge, or into new Best Management Practice (BMP) drainage features. Per TRPA's Parcel Tracker, the site does not currently hold a BMP Compliance Certificate through TRPA.
- Site grading will need to be permitted through a TRPA Grading Permit and demonstrate use of temporary and permanent BMP's for storm water runoff erosion and sedimentation control. Site work involving earthwork not exceeding seven cubic yards is considered Qualified Exempt. Although the proposed site work is expected to exceed that amount, this may influence the sequence of work and permitting.
- Replacing existing landscape areas with new pavement, or covering landscape areas with an extended porte cochere roof, would increase the amount of land coverage as reviewed by TRPA. This site is already over-covered, so removing landscape areas may not be approved unless the coverage is relocated from another portion of the site (i.e. new landscaping is provided elsewhere to compensate).
- Removing trees greater than 14" in diameter will need to be permitted by TRPA.
- Because this site is located within the City limits, all TRPA permit applications need to be submitted to the City review on TRPA's behalf. Dependent on the nature and extent of the proposed work, the City may determine that an application needs to be reviewed by TRPA.

South Tahoe Public Utility District (STPUD):

- Any sewer and waterline lateral alterations, including metering, needs to be designed in accordance with STPUD standards details and guidelines. Currently, the STPUD standard detail for meter boxes governs that they must be located above-ground.

El Dorado County Environmental Management Dept.:

- Decommission/destruction of the existing well would require permitting through the El Dorado County Environmental Management Department. This work needs to be performed by a certified well driller with a C-57 license.
- Removal of any septic tanks or systems would also be under the jurisdiction of the Environmental Management Department and subject to their requirements for removal and disposal.
- At a minimum, well abandonment would involve the following processes:
 1. Preliminary investigation of the well to determine its condition, details of construction, and identification of obstructions that may interfere with filling and sealing.

2. The well needs to be cleared of equipment and other obstructions and cleaned to remove any pollutants and contaminants that could interfere with well destruction operations.
3. The well would need to be filled and sealed as determined based on the conditions that are encountered.

TRPA and the City should be contacted to determine requirements for proper disposal of materials removed from the well. Additional requirements will apply based on materials encountered, condition of the well, and agency-specific requirements.

Any work in the shore zone, such as the proposed Shoreline Protection Modifications to mitigate erosion at the sheet pile wall, will trigger extensive planning, permitting, and environmental requirements. We defer to the Programming Report by Exline & Company for clarification on recommended approach and procedures for shorezone work. It is recommended that the shoreline stabilization work be pursued as a separate, subsequent project to the other planned improvements described in this report.

4. ARCHITECTURAL PROGRAMMING

4.1. Architectural Improvements

Architectural Improvements outlined by the HOA's Special Assessment Projects list (Section 1.2) are provided below. See Section 4.2 for programming and planning considerations as they relate to the list of improvements.

- Revised porte cochere.
- Updated building façade including Facias, siding, windows and doors.
- Updated and compliant exterior lighting and signage.
- Updated and compliant railings and guards.
- Required code upgrades based on findings.
- Preliminary cost estimates.
- Construction schedules using cost per square foot averages and industry standards.

4.2. Programming for Building Improvements

Monarch Architecture has evaluated the existing structure, architectural massing, and function of the resort grounds to identify key deficiencies and propose improvement to the look and function of the property. The improvements have been grouped into 3 main categories exterior cladding, accessibility (ADA), and portico improvements.

Within those categories the following items have been accessed and 3 assorted options for improvements have been included from the minimum recommended level of effort (Option 1), a mid-range upgrade (Option 2), and the most costly/high-end upgrade alternative (Option 3).

- Exterior Cladding. Including siding, floor finish, roof, windows and doors, fascia and trim, exterior accessories attached to building and exterior lighting.
- Portico: The existing portico has been evaluated along with the previous suggestions and report provided by Design Workshop. The descriptions of improvement identified in their report would be similar in architectural character as what was presented by design workshop.
- Accessibility: Including site path of travel, accessible parking spaces, accessible amenities.

4.2.1. Design Components

Design elements required to construct the Special Assessment projects as they relate to the exterior of the building include the following:



Exterior Cladding:

- **Existing Wood Siding:** The current siding is primarily board and batten (Fig. 1a-3a). The current condition appears to have limited issues. There does appear to be a number of locations where patches have taken place and the paint patches are evident (Fig. 2a).
 1. At minimum, repaint building with updated color that is consistent throughout the property. Additional inspection would be done to identify any particular locations of the siding that need any repair and replacement on a case-by-case basis. This

option would be the least cost sensitive but the life span for future work to the siding would more than likely be within the next 7-10 year timeframe.

2. Provide a hybrid in which the majority of the siding remains and is painted and additional accent siding is used to update the front façade and entry of the property. Similar to option one, the maintenance and future work to be completed would be in the 7-10 year timeframe.
3. The costliest option would be to replace the entire siding throughout the property. Various options include a fiber cement siding, stucco, wood siding. This option would have the longest return on future improvements. For instance, fiber cement board offers a 30 year limited time warranty on their siding product Hardie Board.



- **Windows and Doors:** There appears to be a mix of windows and doors throughout the property (Fig. 4a-6a); including vinyl and aluminum. It is our opinion this is due to various improvements over time. Door sills along the accessible path of travel should also be evaluated to make sure they comply with the profile and max ½" vertical rise.

1. At minimum, identify any windows or doors having waterproofing issues and replace. Also identify any doors that do not meet accessibility requirements including door sill issues and replace.

2. In addition to replacing windows and doors that are failing, replace windows and doors on front street façade.



- **Guardrails:** There are three guardrail types currently utilized. The majority looks like it was done in the past 10 years and consists of vertical rebar pickets; this guardrail appears to meet code (Fig. 7a). The other guardrail (Fig. 8a) is located at the guest rooms closest to the lake: These guardrails are not up to date with code and replacement is highly encouraged from a safety standpoint. CBC requires walking surfaces with a 30" or greater elevation drop require a guardrail. The guard is required to be a minimum of 42" high and cannot have any openings that would allow a 4" sphere to pass through. The common use deck and ramps have a 2X2 vertical pickets (Fig. 9a) and appear to be in acceptable condition and meet the code.

1. At minimum, replace the non-code compliant guardrails.
2. Additionally replace the non-code compliant railing, paint the existing railing, and replace the top cap with a new wood piece for aesthetic purposes.
3. Replace all of the railings on the property to match and upgrade the look to help with the general architectural updates of the property.



- Exterior Floor Finishes:** Only the exterior floor finishes were observed. There appears to be a number of different types of floor finishes throughout the property. Concrete, stone pavers and carpet were observed (Fig. 10a-14a). The three main aspects of the flooring that we identified are: accessibility (ADA) requirements, aesthetic and maintenance/failure locations. Properly detailing any replacement of flooring will be critical to ensuring no waterproofing issues in the future.

- At minimum, identify any current waterproofing issues and or failures of flooring and repair. Once a complete path of travel strategy has been developed identify the locations needing upgrades for ADA and remedy those locations only. Note max slope for an accessible path of travel is 2% with no more than a 1/2" rise.
- Complete the minimum for maintenance and ADA and also replace all public area walkway material to be consistent throughout property.
- Replace all flooring at exterior walks, guest balconies and public spaces. Although this option would be the costliest it would ensure a tight seal and help with eliminating future issues.



- **Exterior Lighting:** There were several different exterior light fixtures identified throughout the property. It also appeared that the current exterior lighting does not meet the CSLT and TRPA lighting requirements which effectively requires a dark sky fixture. Lighting appears to be insufficient. There are a number of locations that could benefit from additional lighting. Both from a wayfinding and security standpoint. Exterior Lighting to be replaced/added will also be required to meet California Energy Code requirements.
 1. At minimum, replace the non-code compliant light fixtures and use one consistent fixture type/ style throughout the property.
 2. Replace non code compliant and identify a key number of locations that could utilize additional lighting.
 3. Provide a comprehensive lighting plan that includes landscape, sidewalk, and building lighting throughout the property.



- **Stairs and Handrails:** There are a number of stairs on the property (Fig. 18a-22a). It was observed that none of them meet current codes. Both Accessibility and Life Safety dictate the requirements of stairs including the rise/run, handrail location/profile/height, allowance of open risers, stair width, lighting and warning strips. Once a site plan and as-builts have been completed, it is recommended a complete study of the existing stairs be done and a design completed to update all the stairs on the property.
 1. Only replace stairs that are modified under the special assessment.

2. Bring all stairs up to current codes and provide a consistent guardrail/ handrail design for all stairs.

- **Fascia, Trim and Accessories:** Roof fascia appears to be in good condition. Trim appears to be consistent throughout property. The aesthetics of the trim are unassuming. There are a number of decorative trim pieces that could be removed and redone to match the siding upgrades. It was observed that a number of cable and other accessories have been added to the building over the course of its lifespan. Cleaning up abandoned wires and providing conduit/cleaner solution.

1. At minimum, identify and replace any trim/fascia that is failing.
2. Add additional trim/fascia detailing at front street façade.
3. Additionally, remove all accessories that are no longer being used and replace any that are still being used in conduit located in a discreet location.

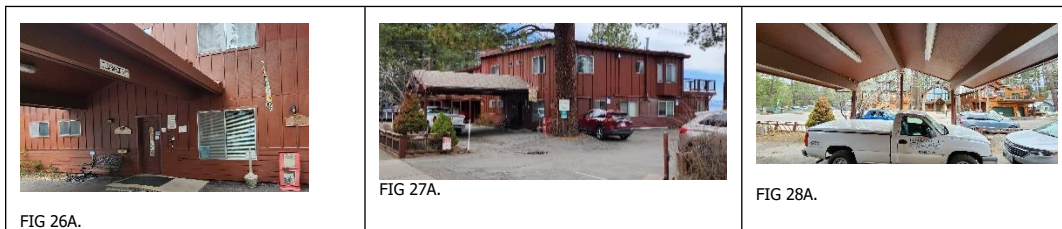
- **Roofs:** A full analysis of the existing roof, including when it needs to be replaced has not been done. With the roof design and limited view. It is recommended that the roof be replaced only if it is needed from a maintenance standpoint. Please note if it does need to be replaced it will need to meet the requirements of the California Energy Code.



- **Upper Deck and Lift Layout:** There is an opportunity to redesign the space where the existing lift and ramp currently exist. It is recommended that there is a reconfiguration of the BBQ Deck area to approve the current aesthetic. Suggestions for creating an outdoor hub to the property include revised seating, fire pits, and redesign of the existing railing.

- **Trash Enclosure:** If the dumpster is to be relocated and a trash enclosure to be constructed, it should be noted that the enclosure will need to be accessible; including a path of travel to the facility and the ability to open door and maneuver in trash enclosure. The cladding of the new trash enclosure should be complimentary to the new cladding selected for the building.

Portico:



- The HOA has identified the Portico (Fig. 26a-28a) as being an important feature to be upgraded; both from a functional and an aesthetic standpoint. A previous study had been conducted by Design Workshop to identify a new design for the entry structure, which was provided to us for review and reference. The HOA Members expressed that they liked the concepts/architecture that had been presented.

The existing structure lacks an expression of architecture. It sheds water/snow over the entry/exit. As identified in this report, the existing portico is sitting in the setback; leaving it in its current location is encouraged. There was also a request from the HOA to create a lock box kiosk for guest arriving after hours. The existing ramp to the entry is not ADA compliant and would need to be redesigned. Once a site plan has been completed a review of how to modify the grades to assist in remedying the issue should be completed.

1. At minimum, add new lighting and cladding to the underside of the existing structure. Provide the late-night guest check in kiosk and redesign the entrance to meet ADA path of travel requirements.

2. In addition to the minimum upgrades, remove the existing roofing and frame and add an additional roof over to shed snow away from the exit/entry. Also clad existing columns and provide some faux architectural elements to give the structure a sense of arrival and expression of architecture.
3. Completely redesign the portico. Study potential of wrapping the portico around the east side of the building and relocating the entrance to the Lobby. Add additional windows into the lobby to make it feel more welcoming with more of a presence.

Accessibility – CBC CH11B & 2010 ADA:

This project is required to meet the CBC CH11B and requirements of the Americans with Disabilities Act (ADA). It is important to note enforcement of the codes is done through litigation. Our findings primarily focus on the exterior of the building. It is highly recommended a complete report be done by a CASP Professional. This portion of the report is going to provide an overview of the existing property and identify items in broad strokes.

In evaluating the existing condition of the property, it is important to understand how the code dictates “Alterations and Additions” when it comes to required upgrades.

Technically infeasible. In alterations where the enforcing authority determines compliance with applicable requirements is technically infeasible the alteration shall provide equivalent facilitation or comply with the requirements to the maximum extent feasible.

- **Guest Room:**

The property currently has one mobility room – Unit L on the ground floor. Access to this unit from the Accessible parking space is provided. There does not appear to be a path from the guest room to the lobby.

The 2019/22 CBC CH11B has changed and now 2 units are required for rooms. One unit with a shower and one with a bathtub. Additionally, 2 guest rooms are required to have communication features in the room. One room can be shared with the mobility

room. It is not known if communication rooms are currently in compliance.

Currently the 1 room is adequate unless: *Per CBC 11B-224.1.1 Alterations: Where guest rooms are altered or added, the requirement of this section shall apply only to the guest rooms being altered or added until the number of guest rooms complies with the minimum number required for new construction.*

- **Parking:**

The current parking lot has one parking space located on the western side of the property. CBC CH11B Requires one Van Accessible parking space for 1-25 spaces.

The space has the appropriate signage. The current location is less than ideal. The code discourages path of travel behind parked cars because of the issue with visibility. Currently there is a path of travel to the Mobility Guest room but no travel to the lobby.

Recommend another location closer to the lobby and a path of travel from the lobby to the guest room be designed.

Please note although not required by code, if an EV Charging Parking space were ever to be incorporated into the site it would also need to be accessible. It is recommended that an EV Space be studied for the project. With the increase in EV Cars, it would be an added amenity for guests.

- **Path of Travel**

Path of travel from the public right of way, Accessible parking space, and to the other site amenities on site will be required. The code requires, at minimum, the path of travel to be updated at areas being altered. Given the scope of the project this requirement will need to be completed. Once a site plan has been completed, the architect and civil engineer should work together to provide a comprehensive strategy for path of travel throughout the site.

- **Signage**

Accessible signage is sporadic on site. Signage code requirements include mounting heights and braille. Compliance of accessible

signage is not difficult to achieve. Suggest updating all on-site accessibility signage with proposed improvements.

- **Passenger Drop Off – Loading Zones**

Included in the Portico redesign at least one passenger drop off – loading zone will be required and should be included in the design.

- **Elevator – Access to Various Levels.**

The current elevator has had numerous issues. It was discussed that providing a ramp and removing the elevator could be a solution. Please refer to Section 3.1.4 for ramp design. There is the additional level of access to the pool and the beach. Various solutions to making these spaces accessible would be extensive. Once a site plan has been completed a study should be done to evaluate elevations and determine if additional ramps or lifts could be incorporated.

4.2.2. Planning and Key Considerations

Title 24 California Code of Regulations

- The California Code of Regulations encompasses the entire suite of volumes that make up the “building code”. Areas include: the Life Safety, Electrical, Plumbing, Mechanical etc. Areas being altered will need to meet the requirements of the most current 2022 Title 24 Code of Regulations. Items that remain and are non-compliant can remain as “existing non-conforming”. The code outlines many of the codes relevant to this project including: Life Safety, Fire Code, Egress, and Envelope design.

California Building Code (CBC) CH11B and 2010 ADA (Accessibility):

- Both National and State accessibility codes apply to this project. When there is a discrepancy between the codes, the most stringent applies. The CBC CH11B dictates most of the codes. Refer to the accessibility section for more detail. It is highly recommended that a CASP Professional be hired to prepare a complete comprehensive report for the property.

California Energy Code (2022 CEC):

- The CEC dictates the Thermal Resistance for the envelope of buildings and lighting efficacy. Areas that are replaced/renovated will need to meet the specific prescribed requirements. For example, any window or door would need to meet certain R values. Any lighting replacement would need to be high efficacy LED fixtures.

Wildland Urban Interface (WUI) (2022 C.B.C. Ch7a)

- The Wildland Urban Interface code is written to prescribe a building envelope that will assist in protecting the building in the event of a wildfire. The scope of work for this building allows it to be exempt from the requirements of the WUI Code CBC 701A.3 Exception 5. Recommend evaluating the scope of siding to be replaced and voluntarily following this code to help protect the building in the event of a future wildfire.

5. SUMMARY

5.1. Tentative Project Sequencing

A tentative sequence of tasks for progressing with the Special Assessment projects discussed in this report is as follows:

Work Item #	Work Item Description
1	HOA identifies select items to pursue from the Special Assessment Projects List
2	HOA enters into Professional Services Agreements for Survey and Design, and Construction Support services
3	Topographic and boundary survey mapping
4	Submit Soils Hydrologic investigation application to TRPA
5	Conduct Geotechnical investigation and soils hydro test pit, if required
6	Verify entitlements and site limitations with Planner and permitting agencies
7	Initiate civil, architectural, and structural design with 30%, 60%, 90% design-level milestones for internal review and coordination
8	Submit plans and applications to permitting agencies
9	Address agency review comments
10	Solicit contractors for bidding, identifying any required phasing
11	Construction (TRPA grading season window: May 1 – Oct. 15)
12	Inspections and material testing as required
13	Final walk-through, punch list, and permit close-out process

5.2. Tentative Design Sequencing

Preliminary Design	Complete Site Survey, Survey Building and create as-built drawings.	4-6 Weeks
Schematic Design	Identify the elements and direction the HOA wants to pursue and provide Conceptual Drawings including Site Plan, Floors Plans and Elevations	6-8 Weeks
Design Development	Engage additional Consultants as determined by scope. Prepare Entitlements package for TRPA and CSLT	4-5 Weeks *Concurrently Complete Construction Document Phase while the Entitlement package is under review.
Construction Document Phase	Complete Construction Documents Adequate for Submittal to Jurisdictions and for Contractor Bidding	8-10 Weeks
Bidding and Permitting Phase	Concurrently Bid project while under review with Jurisdictions.	8-12 Weeks

5.3. Provisional Budgetary Estimate

Appendix A includes preliminary cost opinions from Lumos & Associates for civil and structural related components and from Monarch Architecture for architectural components. These costs are based on preliminary assumptions and project discussions with HOA representatives.

APPENDIX A

Preliminary Opinion of Probable Costs

Client: The Beachcomber Inn Vacation Membership Association
Project Name: Beachcomber Inn Improvements
Report: Budgetary Construction Cost Opinion
Date: 07/21/2023



Budgetary Construction Cost Opinion					
Item No.	Description	Unit	Quantity	Unit Cost	Cost Estimate
1	Parking Lot Improvements				
1.2	Parking Lot / Driveway Reconfiguration and Repaving	SF	12,000	\$ 75.00	\$ 900,000.00
1.3	Utility Service Line Replacements	LF	250	\$ 80.00	\$ 20,000.00
1.4	Well Abandonment	LS	1	\$ 15,000.00	\$ 15,000.00
1.5	3 ft Tall Stone Landscape Wall	SF	285	\$ 45.00	\$ 12,825.00
2	Retaining Wall				
2.1	Retaining Wall w/ Integral ADA Ramp	LS	1	\$ 1,600,000.00	\$ 1,600,000.00
3	Carport and Supporting Elements				\$ 82,940.00
3.1	Construction Costs	LS	1	\$ 27,500.00	\$ 27,500.00
4	Common Deck Area Rebuild				\$ 55,440.00
4.1	Full Rebuild of Deck	SF	792	\$ 70.00	\$ 55,440.00

Construction Cost SubTotal = \$ 2,769,145.00

Engineering & Survey (20%) \$ 553,829.00

Contingency (30%) \$ 996,892.20

Project Total = \$ 4,319,866.20

General Note:

This preliminary estimate of probable construction cost is the Engineer's best judgement as a professional engineer generally familiar with this type of construction. However, since the Engineer has no control over market conditions, the Engineer does not guarantee that proposals, bids, or actual construction cost will not vary from this estimate.



Budgetary Architectural Cost Estimate				
Category	Description	Quantity	Unit Cost	Cost Estimate
Exterior Cladding				
	Replace all siding on property.	16,000 sq.ft.	50.00/ sq.ft. new siding	800,000
Windows and Doors				
	Replace all Doors and Windows on property	120 doors/windows	6,000/ Door or window	720,000
Guardrails				
	Replace all Guardrails.	840 lin.ft.	200/lin.ft.	168,000
Exterior Floor Finishes				
	Replace all Exterior walking surfaces.	6200 sq.ft.	40/sq.ft.	248,000
Exterior Lighting				
	Update lighting throughout site including landscape lighting, stair lighting and walkway lighting.			120,000
Stairs and Handrails				
	Replace Handrails			75,000
Fascia, Trim and Accessories				
	All new fascia and trim and clean up exposed accessories and wires, provide concealed conduit.			120,000
Upper Deck and Lift Layout				
	Reconfigure and update			75,000
Trash Enclosure				
	Build new trash enclosure.			35,000
Portico				
	Redesign Portico. Reconfigure front office with new windows/door entry.			350,000
Subtotal	30% Contingency	Contractor P&O (18%)	Total	
2,711,000	408,300		306,225	3,425,525

Note: The cost estimate is provided based on assumptions/knowledge of local building construction conditions at a particular point in time. Building costs and conditions can vary with time. As the design is further developed, additional estimates should be completed. Costs are based on Option 3 for all items listed in Section 4.2 of the Programming Report.

Professional Fees:		Note: Fee will be dependent on extent of determined improvements.
Architect	125,000-185,000	
As Built Meas	25,000	
Electrical Engi	6,000	
CASP Professi	8,000	

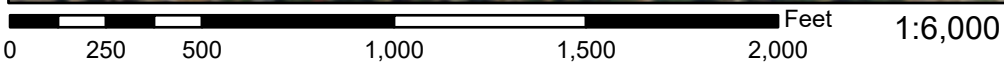
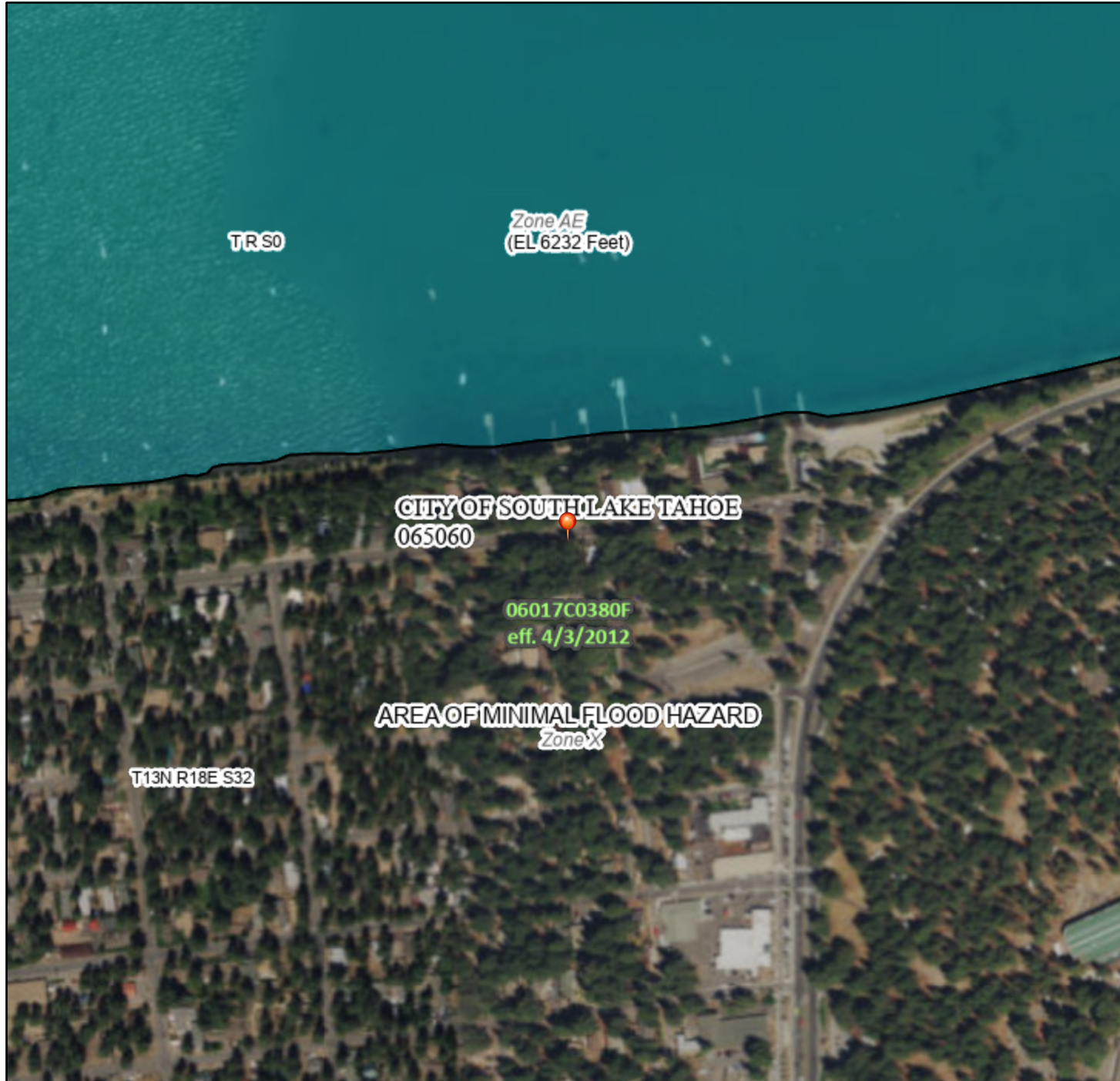
APPENDIX B

FEMA National Flood Hazard Layer FIRMette

National Flood Hazard Layer FIRMMette



119°59'5"W 38°56'53"N



119°58'27"W 38°56'25"N

Basemap: USGS National Map: Orthoimagery: Data refreshed October, 2020

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

- | | | |
|------------------------------------|--|--|
| SPECIAL FLOOD HAZARD AREAS | | Without Base Flood Elevation (BFE)
<i>Zone A, V, A99</i> |
| | | With BFE or Depth <i>Zone AE, AO, AH, VE, AR</i> |
| | | Regulatory Floodway |
| OTHER AREAS OF FLOOD HAZARD | | 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile <i>Zone X</i> |
| | | Future Conditions 1% Annual Chance Flood Hazard <i>Zone X</i> |
| | | Area with Reduced Flood Risk due to Levee. See Notes. <i>Zone X</i> |
| | | Area with Flood Risk due to Levee <i>Zone D</i> |
| OTHER AREAS | | NO SCREEN Area of Minimal Flood Hazard <i>Zone X</i> |
| | | Effective LOMRs |
| | | Area of Undetermined Flood Hazard <i>Zone D</i> |
| GENERAL STRUCTURES | | Channel, Culvert, or Storm Sewer |
| | | Levee, Dike, or Floodwall |
| OTHER FEATURES | | 20.2 Cross Sections with 1% Annual Chance Water Surface Elevation |
| | | 17.5 Coastal Transect |
| | | Base Flood Elevation Line (BFE) |
| | | Limit of Study |
| | | Jurisdiction Boundary |
| | | Coastal Transect Baseline |
| MAP PANELS | | Digital Data Available |
| | | No Digital Data Available |
| | | Unmapped |



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **2/7/2023 at 3:48 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

APPENDIX C

Structural Investigation Summary and Structural Evaluation Report



Reno
950 Sandhill Road, Suite 100
Reno, Nevada 89521
775.827.6111

April 25, 2023
Job No. 10923.000

Mr. Phil Lee
Beachcomber Inn Resort HOA
999 Lakeview Ave.
South Lake Tahoe, CA 96150

SUBJECT: STRUCTURAL INVESTIGATION SUMMARY AND STRUCTURAL EVALUATION REPORT

Dear Mr. Lee:

On November 30 2022, a structural condition survey was conducted at the Beachcomber Inn Resort located in the city of South Lake Tahoe, CA 96150. The purpose of this assessment was to investigate condition of various existing structural components and provide our professional opinion of feasibility of proposed site improvement plans. Our site investigation included review of existing construction documents and a visual survey of following items:

- Carport and supporting elements.
- Previously unpermitted common deck area.
- Walkway railing attachments
- Elevator support walls
- Stepped retaining walls
- Existing Porte Cochere

Summary of Structural Findings

CARPORT AND SUPPORTING ELEMENTS

The south face of the building has a series of carports on the first floor. The carports' southern edge is supported by steel columns or CMU wall and they span approximately 10 feet. These beams provide support for the walkway that leads to the rooms on the second floor, as well as the 4x4 wooden posts that support the roof, as shown in Photo 1. The timber beam was measured approximately 5 inches in width, while the supporting steel posts are 6 inches square. The upper timber posts that supported the roof are 4x4 posts. These measurements were later verified by repair drawings by Jones & Associates Civil Engineer, dated 1/4/2000, and Job #9911-05.

Based on the design criteria outlined in CBC 2022 and snow loads from City of Lake Tahoe's design criteria, we assessed the capacity of the building's beams and columns. We determined that both the 4x4 timber posts above the beam and the 6x6 steel column supporting the beam have sufficient capacity to support the imposed loads. However, the 5 1/8" x 7 1/2" Glulam beams exceeded the allowable bending capacity under design snow loads and we recommend strengthening the beam.

ROOFTOP DECK

The building located North of the retaining wall has a rooftop deck. The wooden deck structure is comprised of deck boards laid flat on wood framing members below the deck boards. The framing members beneath the deck were not visible, but appeared to be supported by the original roof, see Photo 2. Based on the visible nailing pattern, we estimate that the deck boards span approximately 2'-6" assuming a supporting member is present under the nails, see Photo 3.

We have evaluated the deck boards for live loads and snow loads per the State of South Lake Tahoe requirements. We also discussed the property's record with Mr. Robert Townsend and learned that the observation deck was last replaced on 09/03/2004. Design parameters for the deck were selected from the 2001 NDS specification supplement by the American Wood Council, using values for DF#2.

Based on the assumptions stated above, the timber deck has adequate capacity to resist applied loads. However, we understand that the deck area has not been permitted and would need to be permitted. Additional investigation is necessary to determine if the framing member below is adequate and if it is properly attached to the existing roof.

RETAINING WALL CONDITION

There is an existing retaining wall system, comprising of two masonry retaining walls and one timber retaining wall located between the upper and lower building areas. The lower masonry wall is approximately 8 feet tall, while the intermediate masonry wall is approximately 6 feet tall. The lower masonry wall is approximately 9 feet away from the lower building, and there is a slab-on-grade with two drainage opening between the lower wall and the building, see photo 4. The upper timber retaining wall is approximately 3 feet tall.

During measurement, it was discovered that the lower retaining wall is out of plumb by approximately 9 inches. There is a horizontal crack running parallel to the top of the wall, and this appears to be caused by the wall deflecting and being restrained at the top by the concrete slab, as shown in Photo 5. Although weep holes are present, we did not observe any signs of drainage through the weep hole. The absence of any discoloration on the wall's paint suggests that there is no active flow of moisture, as can be seen in Photo 6. At the west end of the intermediate retaining walls, a tilting of approximately 2 inches was found. While it is possible that the tilting is greater in the middle of the wall, it was inaccessible for verification. We did not observe any cracks or spalls that would indicate structural distress.

EXISTING PORTE COCHERE

The building's south entrance currently features a Porte Cochere as shown in Photo 7, which is supported by three timber beams spanning in the North-South direction. The south end of the beam is supported by round metal columns and the North end of the beam spans into the building. Based on the conversation with the representatives from the resort, beam extends further into the building, but the actual support condition on the North end of the beam could not be identified.

MISCELLANEOUS ITEMS

To evaluate the property's existing condition, we conducted a thorough walk-around, assessing the state of the deck, railing attachment, elevator, and its surrounding areas, as well as searching for any other structural issues.

During our assessment, we did not identify any significant damage to the deck walkways and railing attachments. However, we did note some loose deck connections and deflecting railings that will require maintenance.

We performed a visual inspection of the elevator system and the surrounding CMU walls. The elevator system seems to be a proprietary system, and we did not detect any signs of distress on the structural members or the surrounding CMU walls.

Additionally, we observed spalling on the CMU walls at the North-East corner of the pool deck, as shown in Photo 8. This issue seems to be localized to that specific area, and we did not witness any similar issues in that vicinity.

CONCLUSIONS AND RECOMMENDATIONS

CARPORT AND SUPPORTING ELEMENTS

The glulam beam supporting the deck needs to be strengthened. We recommend reinforcing the existing beams by attaching metal plate or wood members, preferably 2x6's, to the side of the existing glulam beams to provide the necessary capacity. This approach would be the most cost-effective and least intrusive solution. Although the beams supporting the roof rafters were inaccessible for evaluation, we anticipate a similar deficit in capacity regarding the beam supporting the roof and recommend including it in the scope of repair.

All other members and connections appear to be in good condition and do not require immediate attention.

ROOFTOP DECK

Based on our inspection, the visible framing members on the rooftop deck appear to comply with current design codes and load requirements. However, since we were unable to access the underlying supporting members and conditions, we require further information to determine if the existing system conforms to current design codes. To ensure compliance, we can conduct additional investigations to verify the existing capacity of the members and propose remedial measures if necessary.

RETAINING WALL CONDITION AND FEASIBILITY OF NEW WALL SYSTEM WITH ADA RAMP

The lower retaining wall is exhibiting signs of excessive deflection, which is not permissible under code provisions unless a comprehensive analysis, including the tilt, is conducted. Due to limited information regarding the wall's original construction and reinforcement, it is difficult to estimate the capacity at this point. However, the wall appears to be functioning adequately under normal conditions, despite its noticeable tilt for a considerable time. It is reasonable to assume that the wall can withstand the earth pressure to which it is subjected at this point. However, the retaining wall at this location can be subjected to additional loads from snow surcharge, hydrostatic pressure, and seismic thrust. If the weep holes are not effective, any hydrostatic pressure can severely impair the functionality and the capacity of these walls.

At this point, we advise monitoring the retaining wall's tilt frequently for any new indications of damage. Additionally, as a permanent solution we recommend replacing the current system with an engineered retaining wall system. As part of this replacement plan, we have explored the possibility of integrating an ADA accessible ramp into the current retaining wall location. We have worked with Brian Martone from Monarch Architecture to develop a concept of the proposed ramp, see figure 1.

Structurally, the new system will require demolishing the existing walls, elevator walls, and installing a new tied-back full height retaining wall adjacent to the upper building, see figure 2. A tied back retaining wall consists of a concrete wall supported by helical anchors or grouted tiebacks. This system utilizes the mass of soil behind the wall to stabilize it, eliminating the need for a large foundation. While the new full-height wall is being built, existing retaining walls can be used to stabilize the soil mass. The tied-back wall will isolate most of the lateral earth pressure from the ramp walls. The ramp walls will have a tapered top to match the slope of the ramp. To determine the appropriate depth of penetration for these tied-back walls, a comprehensive geotechnical study will be required. The dimensions for the current concept are approximate and are based on field measurements and topographic survey drawings by TA Turner and Associates, project number 9012, and dated 06/18/1990. We also recommend conducting a 3D survey of the area to carefully design the horizontal and vertical layout of the ramp and ensure that all necessary clearances are met.

In summary, we have determined that the existing site has adequate space to accommodate new retaining walls with the ramp. The alternative is to design a traditional cantilevered retaining wall with a large footing. The final design will depend significantly on the geotechnical properties of the existing site and the design of the tied-back system, as well as any potential environmental impacts in the Tahoe Basin. Careful planning will be necessary for either of these alternatives during the

demolition phase, which may involve temporary retaining systems to protect existing structures. Once the survey and geotechnical information is received, we can provide additional assistance with the planning and preparation of final design documents and demolition plans.

EXISTING PORTE COCHERE EXPANSION

We understand that the modification plan includes expanding the Porte Cochere by extending it further south. We have evaluated the following options for expansion:

1. Remove the existing metal columns and splice the existing wood beam with metal plate. Provide three new metal column at the south end to support the extended framing members.
2. Cut the existing beam at the building's exterior wall and redesign the new Porte Cochere as necessary.

Based on our evaluation, we recommend the second option for the following reasons. Option 1 would result in heavy connections that may not blend with the architectural concept. Additionally, it was also discovered that the existing beam extends into the building. Therefore, option 2 will prevent new loads from being transferred into the existing building.

MISCELLANEOUS ITEMS

The spalled CMU, see Photo 8, wall may allow moisture to penetrate the wall, which can lead to secondary issues such as corroding rebar or increased hydrostatic pressure on surrounding wall members if proper drainage is not present. We recommend inspecting for cracks and additional damage and repairing the spalled wall using a grout or similar product rated for repairs.

To restore capacity, damaged, loose, and corroded nails and screws on the deck and railing members should be replaced with similar materials. We recommend using galvanized or stainless steel connectors for all new wood framing connectors to prevent corrosion-related issues. Wood members that are not pressure treated should be painted or stained to prolong their usable life.

If a decision is made to proceed with the construction of the ADA compliant ramp, the existing elevator system will be demolished and no additional evaluation is required. If the elevator system is to remain, we will need to conduct further evaluation to verify all connections between the elevator system and surrounding system to ensure there are no structural issues.

Other site improvement items such as utilizing a planter retaining wall and adding new stairs are minor in nature and feasible, as long as there is enough clearance for these items. These items can be incorporated into the scope as part of the future design.

Limitations

Please note that our findings are based on a limited field inspection and visual observation of readily accessible structural components. While we have endeavored to provide a comprehensive assessment, it is possible that there are hidden conditions, which, if encountered, might affect our conclusions.

If you have any other questions regarding our findings, please do not hesitate to contact me at (775) 827-6111.

Sincerely,
LUMOS & ASSOCIATES

Manish Khanal, P.E.
Project Engineer - Structural Engineering Division
California Civil Engineer C94298



04/25/2023

PHOTOS

CARPORT AND SUPPORTING ELEMENTS



Photo 1 – Carport looking north



Photo 2 – Rooftop Deck



Photo 3 – Deck attachment



Photo 4 – Lower Retaining Wall

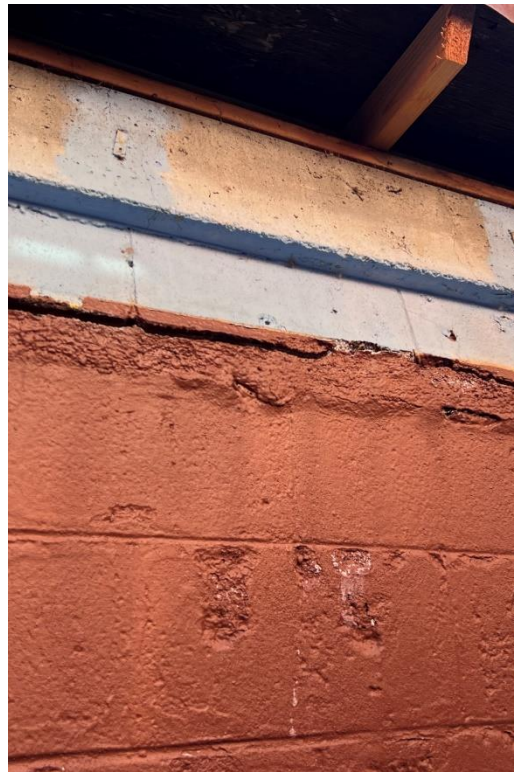


Photo 5 – Horizontal crack along the top of wall



Photo 6 – Weep hole without evidence of moisture



Photo 7 – Existing Porte Cochere

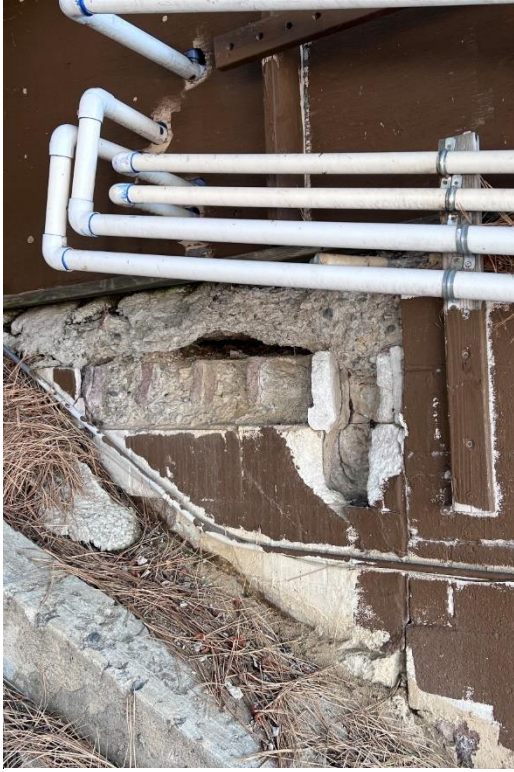


Photo 8 –Spalling CMU wall on
North-East corner of Pool Deck

FIGURES



9222 Prototype Drive
Reno, NV 89521
(775) 827.6111

178 South Maine Street
Fallon, NV 89406
(775) 423.2188

308 N. Curry Street, Ste 200
Carson City, NV 89703
(775) 883.7077

PO Box 890
312 Doria Court, Ste 202
Zephyr Cove, NV 89448
(775) 588.6490

Client: BEACHCOMBER INN

Description: PROPOSED RAMP PLAN

By: MK

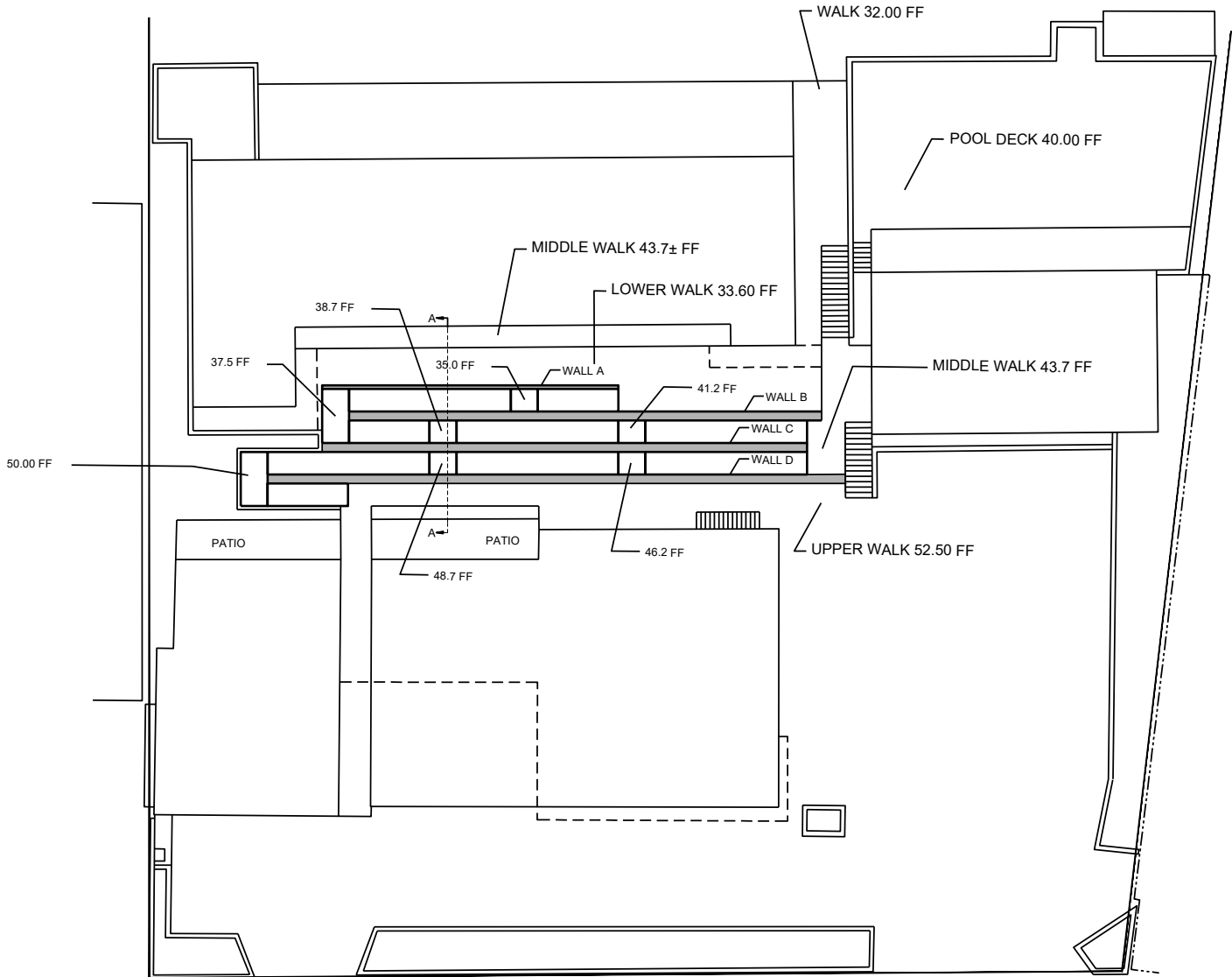
Checked By: _____

04/25/2023
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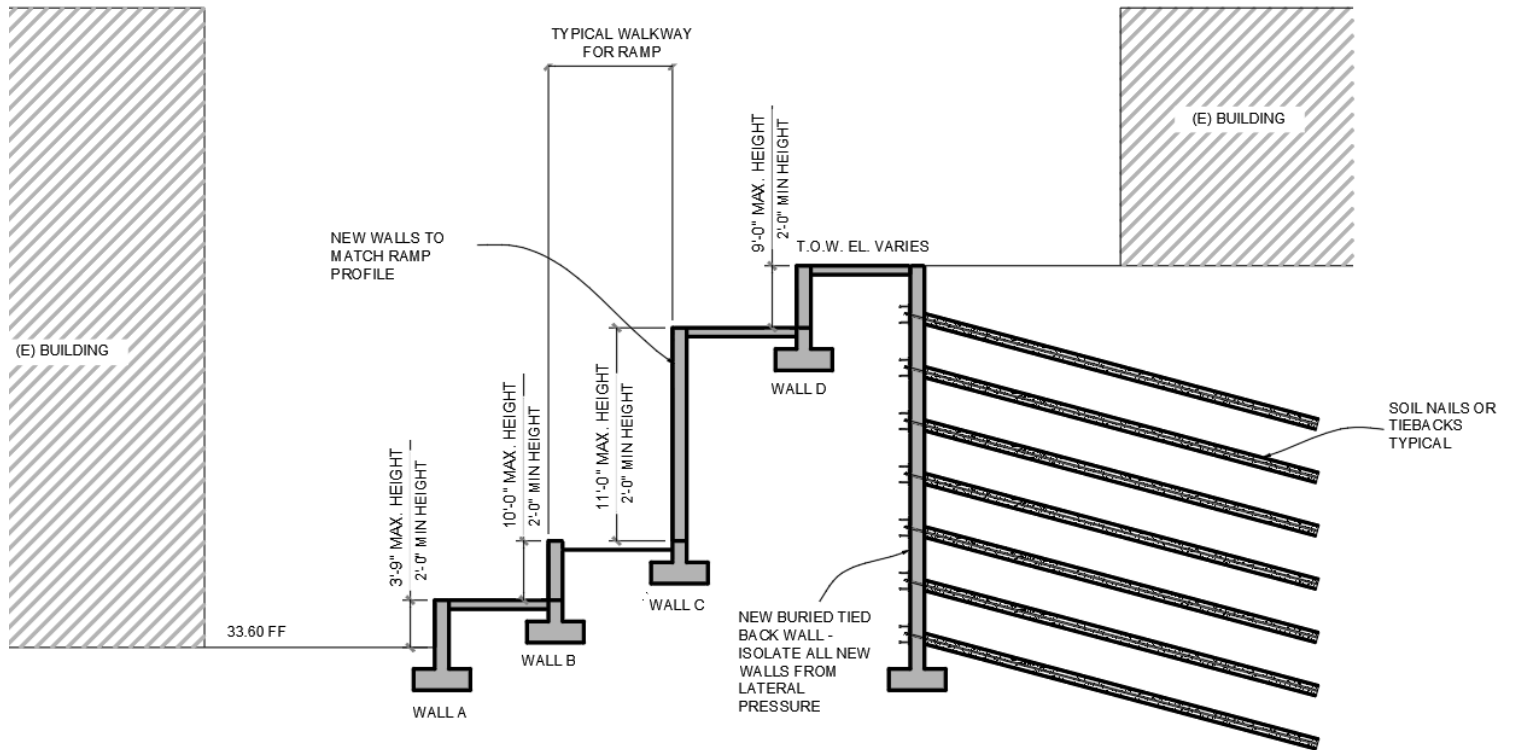
Job No. 10923-000

Date: 4/7/23

Date: _____



**FIG 1 : PROPOSED PLAN VIEW OF NEW
RETAINING WALLS WITH RAMP**



SECTION A-A

**FIG 2 : PROPOSED SECTION VIEW OF NEW
 RETAINING WALLS WITH RAMP**